

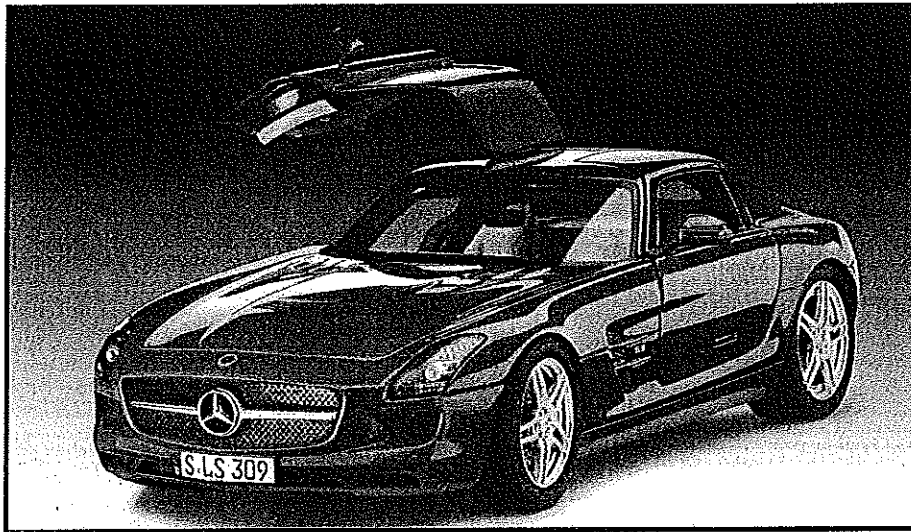


Mercedes-Benz SLS AMG

07100 - 0389

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Mercedes-Benz SLS AMG

Der Mercedes-Benz SLS AMG wurde im September 2009 erstmalig öffentlich präsentiert. Das erste von AMG eigenständig entwickelte Fahrzeug erinnert mit dem breiten Grill und seinem großen Stern, den markanten Flügeltüren und den Kiemen hinter den vorderen Kotflügeln an das legendäre SLR Coupé bzw. den 300 SL aus Mitte der 50er Jahre. Doch der Zweisitzer ist mit dem zeitlosen Retrodesign nicht nur optisch eine Faszination, sondern auch technisch. Das Herzstück dieses Ausnahmefahrzeugs schlummert unter der langen Motorhaube; der weltweit stärkste serienmäßige Achtzylinder-Motor mit 6208 ccm Hubraum bietet sensationelle 571 PS bei 6800 U/min. Bei der Weiterentwicklung des intern M159 genannten V8-Triebwerks wurde bewusst auf eine Ölwanne verzichtet. Statt dessen wird der Motor des SLS aus einem separaten Trockensumpftank mit Öl versorgt, was einen sehr tiefen Einbau nahe an der Vorderachse ermöglichte. Dadurch besitzt der Flügeltürer eine geradezu perfekte Gewichtsverteilung (47 Prozent auf der Vorderachse), was der Traktion und der Kurvenlage zugute kommt. Zudem kam bei dem Rennsportwagen für die Straße konsequente Leichtbauweise zum Einsatz. So besteht das Chassis, ebenso wie die Karosserie, aus Aluminium und wiegt gerade einmal 241 Kilogramm.

Der leistungsstarke Motor mit einem maximalen Drehmoment von 650 Newtonmetern sorgt in Kombination mit dem Siebengang-Doppelkupplungsgetriebe und wegen des geringen Fahrzeuggewichts für eine Atemraubende Performance. Sofort nach dem Start lässt das tiefe Grollen erahnen, welche Kraft in diesem Supersportwagen ruht. Dennoch wird man von dem Fahrverhalten mehr als überrascht. Nicht wegen des Sprints von 0 auf 100 Stundenkilometern, der in 3,8 Sekunden gelingt, oder der Höchstgeschwindigkeit von 317 km/h. Vielmehr überzeugt der SLS durch extrem schnelle Gangwechsel, seine agilen, unglaublich schnellen Reaktionen auf kleinste Gaspedalbewegungen und den leichtfüßigen, aber stets berechenbaren Querschleunigungen in den Kurven. Doch trotz dieser begeisternden Rennsportigenschaften wird nicht auf Komfort verzichtet. Das Cockpit bietet bequeme Sitze und dank der vier wählbaren Programme kann in mehreren Abstufungen von rein sportlichem bis komfortablem Fahren gewählt werden. Der puristisch eingerichtete Innenraum erinnert mit einem dem Schubregler eines Düsenjägers ähnlichen Schaltknopf, den zentral angeordneten Instrumenten in Piloten-Optik und den vier Strahltriebwerken gleichenden Lüftungselementen tatsächlich an ein Flugzeugcockpit, lässt aber auch Eleganz und Luxus nicht vermissen.

Fazit: der Flügeltürer setzt mit seinem Mix aus Leistung, Fahrverhalten und Design schon jetzt Maßstäbe und bietet alle Voraussetzungen dafür, bald eine Sportwagen-Ikone zu werden und Automobilgeschichte zu schreiben.

Mercedes-Benz SLS AMG

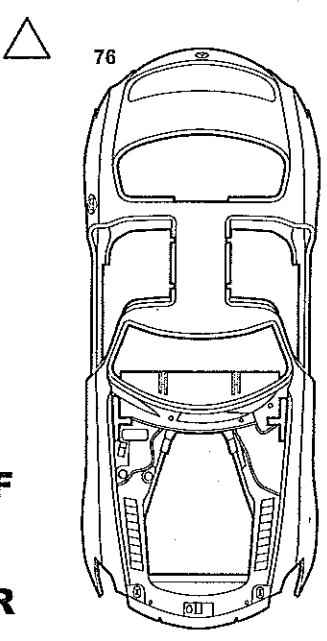
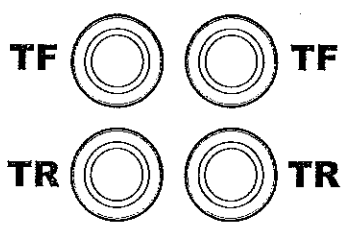
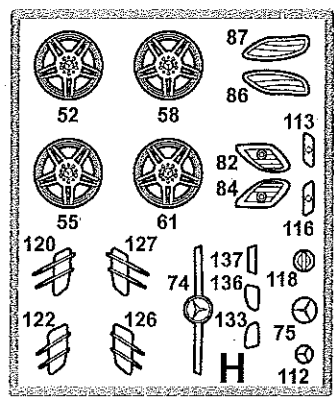
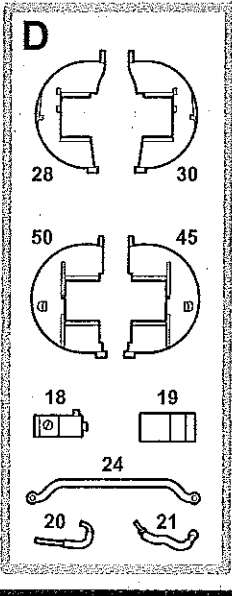
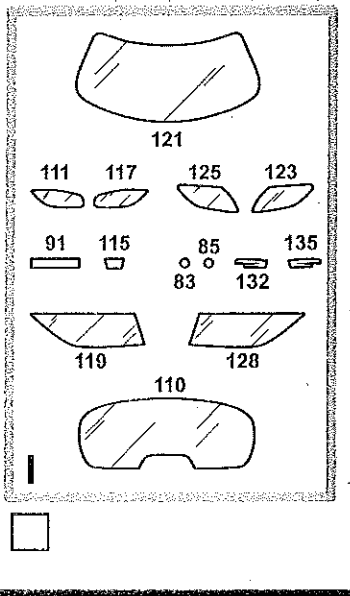
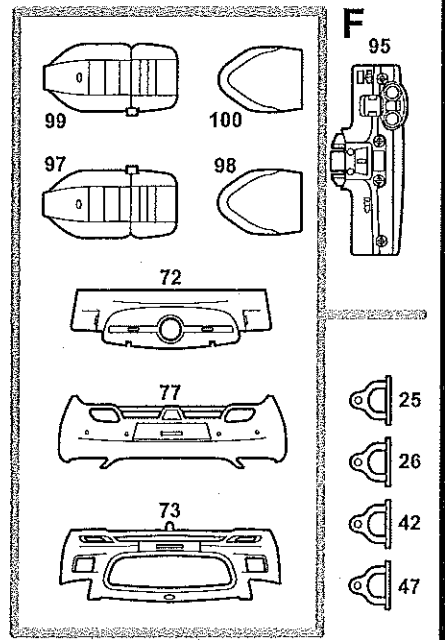
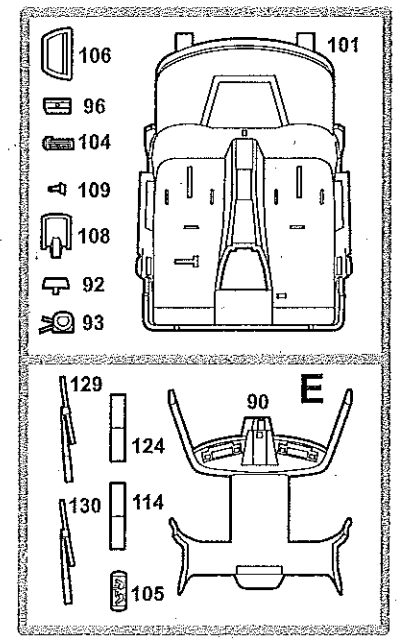
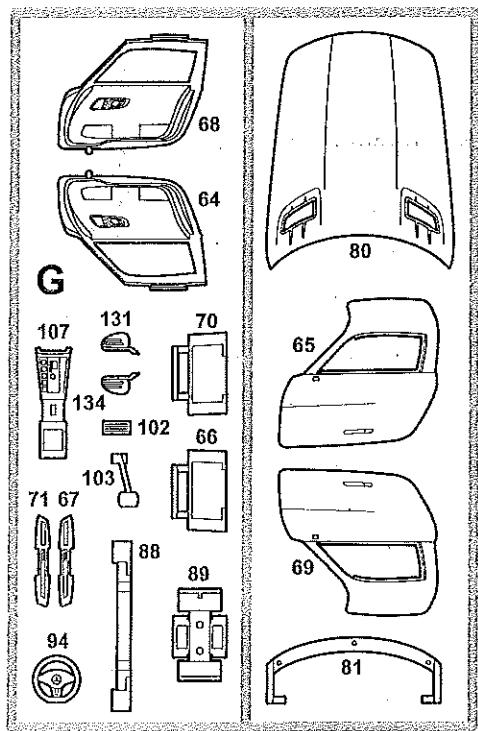
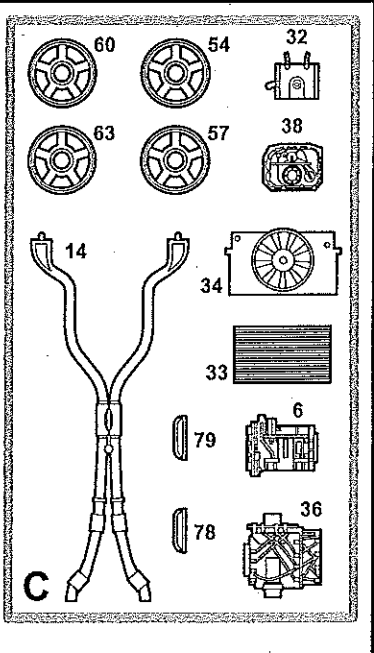
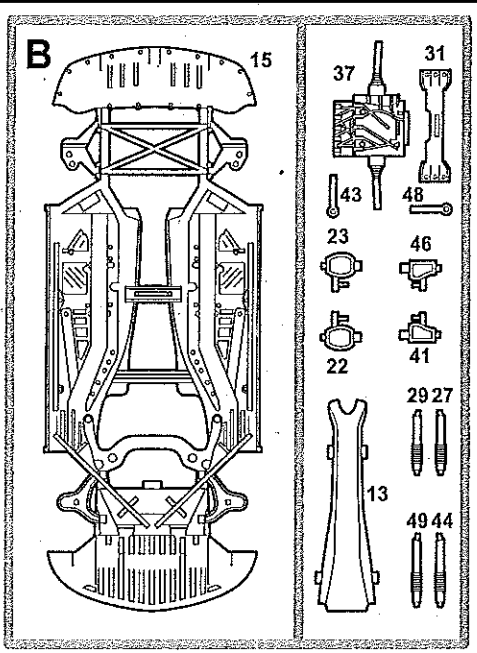
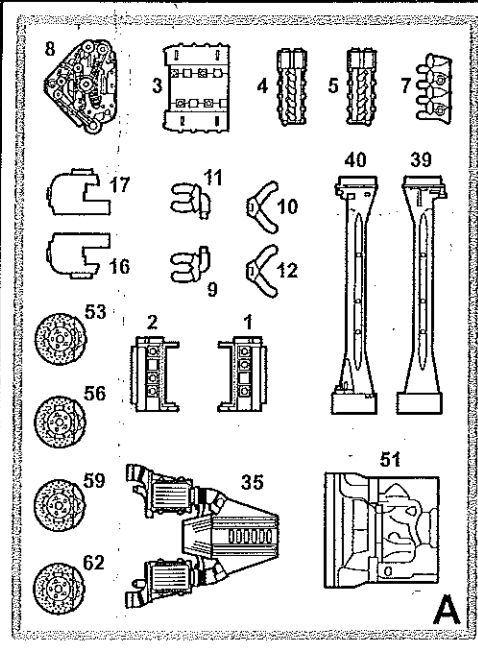
The Mercedes-Benz SLS AMG was presented to the public for the first time in September 2009. With its wide radiator grille and large Mercedes-Benz star, striking gullwing doors and fins mounted behind the front fenders, the first clean-sheet design developed by AMG is reminiscent of the legendary SLR coupé or the 300 SL from the mid 1950s. But the fascination of the two-seater with the timeless retro design isn't just about the aesthetics but involves the technical features too. The heart of this exceptional vehicle beats from under the long bonnet. The world's most powerful series eight-cylinder 6208 cc engine generates a sensational 571 hp at 6,800 rpm. On developing the V8 engine, bearing the internal designation M159, further, a conscious decision was made not to use an oil pan. Instead the SLS's engine is supplied with oil from a separate dry sump tank, thus allowing it to be mounted very low down in the vehicle close to the front axle. As a result the gullwing sensation has an almost optimum weight distribution (47 percent on the front axle) that benefits both the traction and cornering performance. What's more, the road version of the racing sports car made consistent use of lightweight construction. Thus both the chassis and the bodywork are made of aluminium and weigh in at just 241 kilos.

Combined with the seven-speed double clutch transmission and on account of the low vehicle weight, the powerful engine with a maximum torque of 650 Nm puts in a breathtaking performance. As soon as the engine starts up, the deep roaring sound testifies to the power that this super sports car is about to unleash. The car's road performance and ease of handling therefore come as quite a surprise. But the convincing features of the SLS are not so much the acceleration from zero to 100 km/h (60 mph) in just 3.8 seconds or the top speed of 317 km/h (200 mph), but rather the extremely rapid gear-shift, the agile, unbelievably quick response to the slightest touch of the gas pedal and the "light-footed" but always predictable lateral acceleration on cornering. Yet despite these exciting racing sport characteristics there is no curtailment of comfort. The cockpit offers comfortable seats and thanks to the four selectable programmes there is a choice of several different levels ranging from outright sporty to downright comfortable. With a gear stick resembling the throttle control of a jet fighter, the centrally arranged pilot-look instruments and the four jet turbine-like air vents, the puristically designed interior is truly reminiscent of an aircraft cockpit, yet lacking nothing in terms of luxury and elegance.

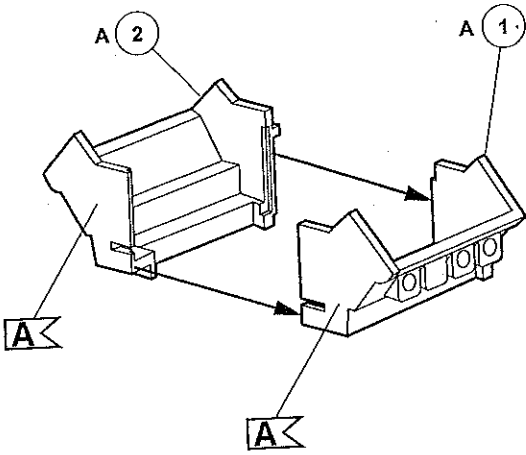
Conclusion: the gullwing wonder with its mix of performance, handling and design already sets standards and is destined to soon become a sports car icon that may well write automobile history.

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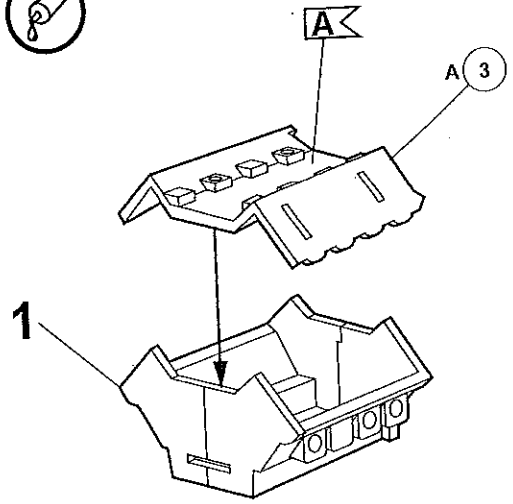
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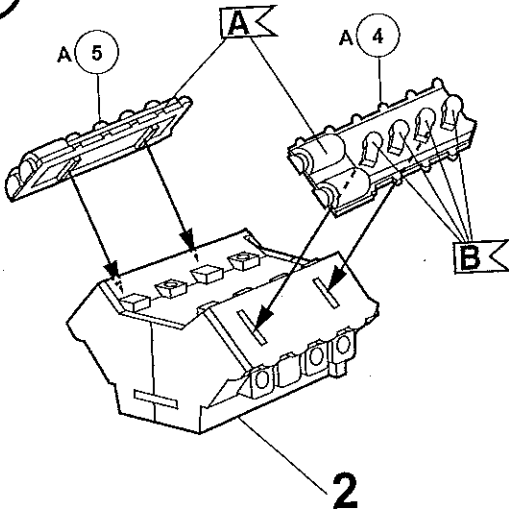
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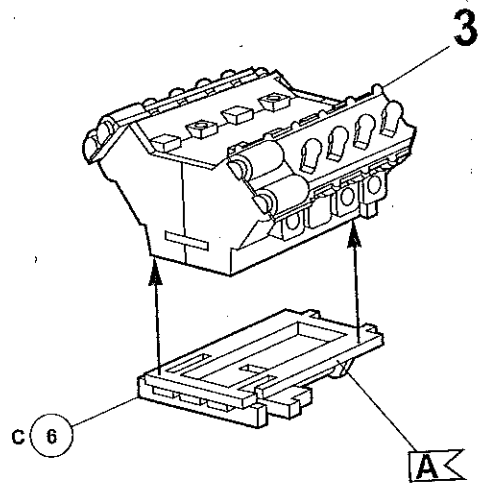
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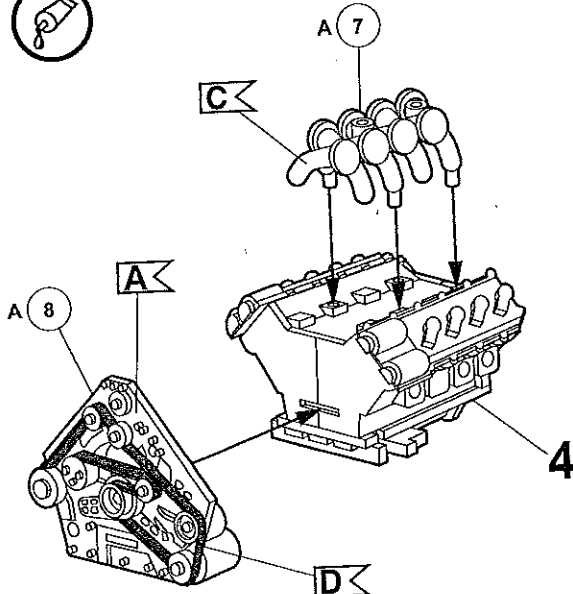
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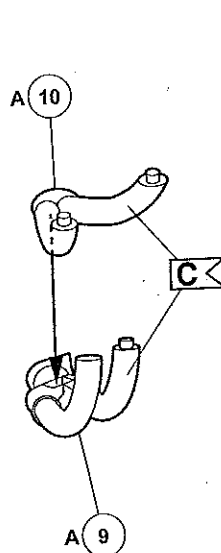
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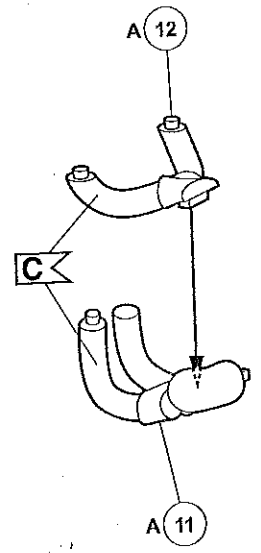
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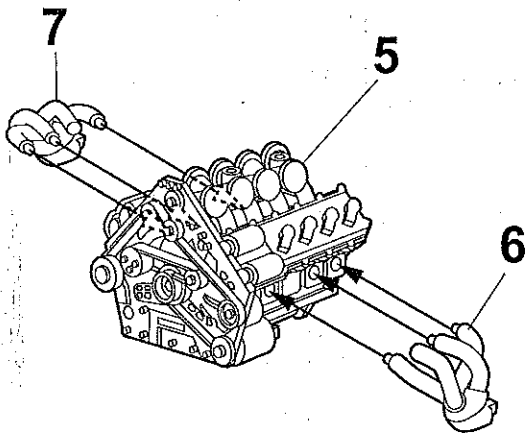
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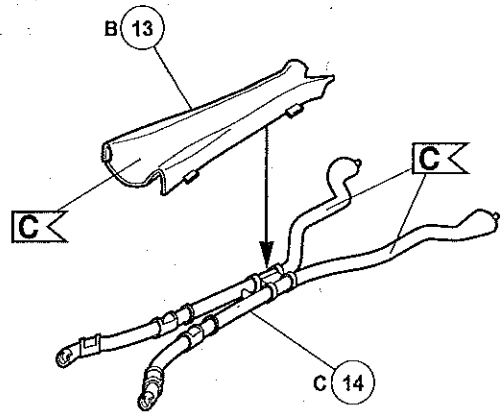
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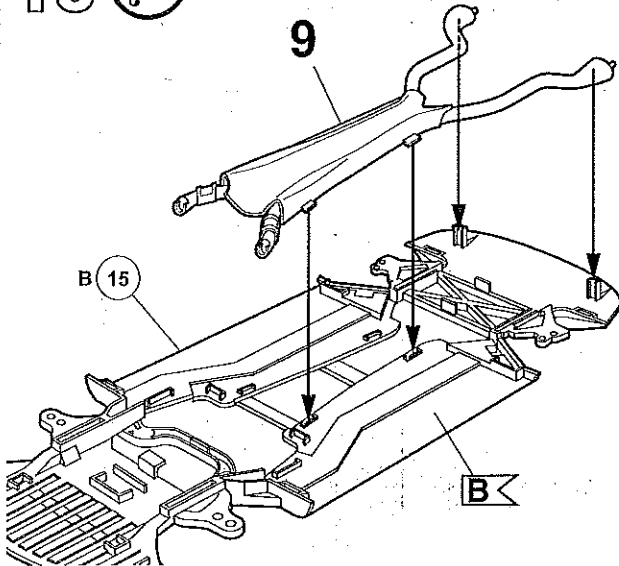
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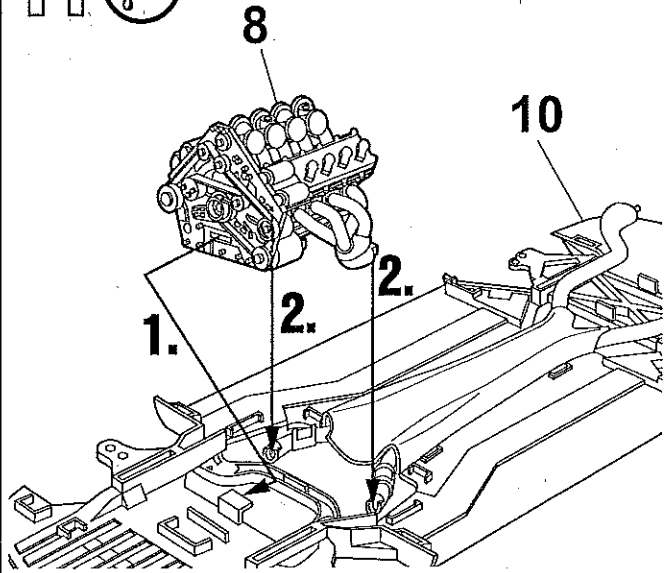
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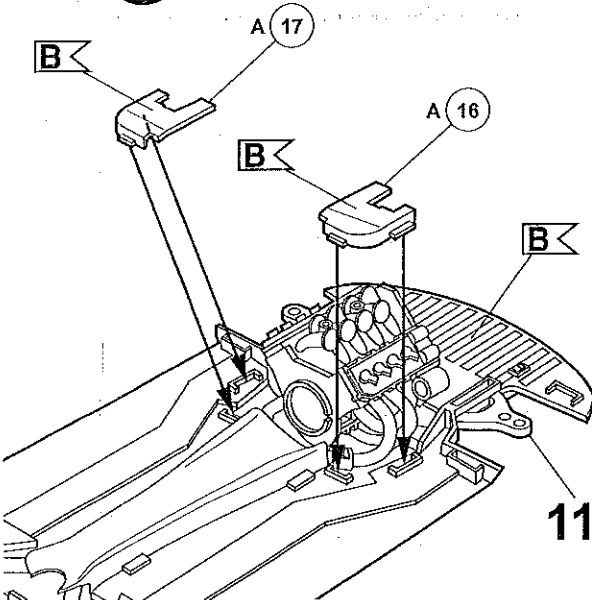
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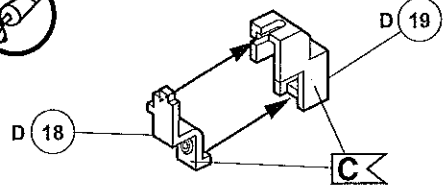
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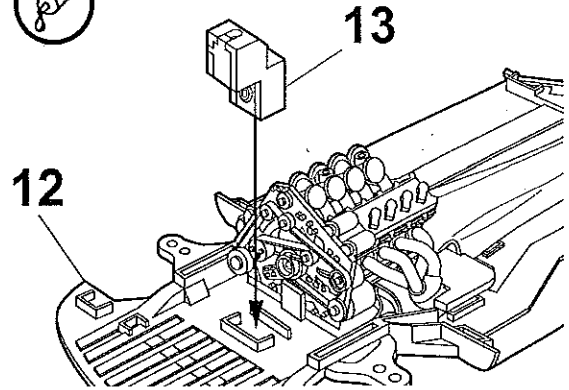
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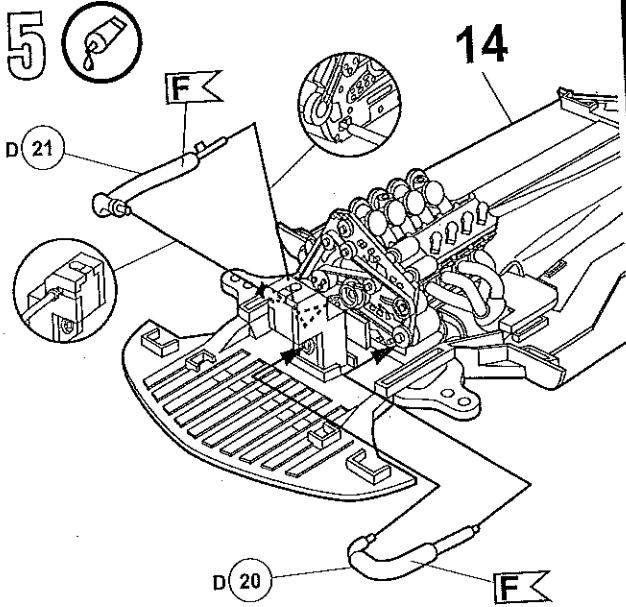
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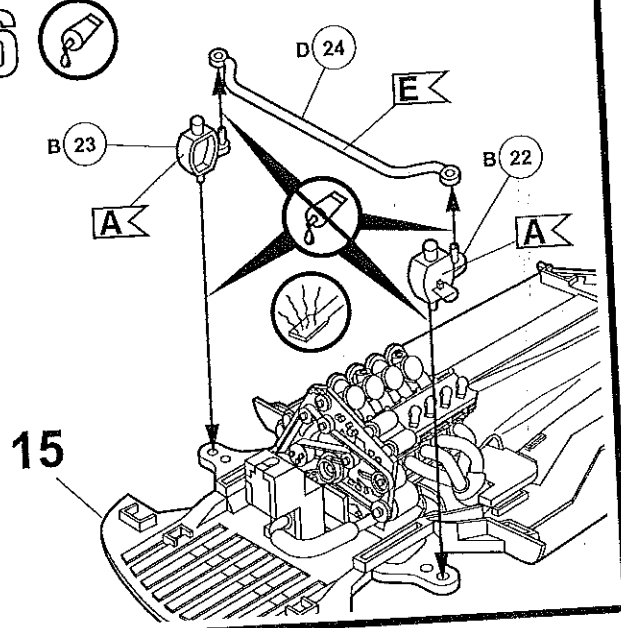
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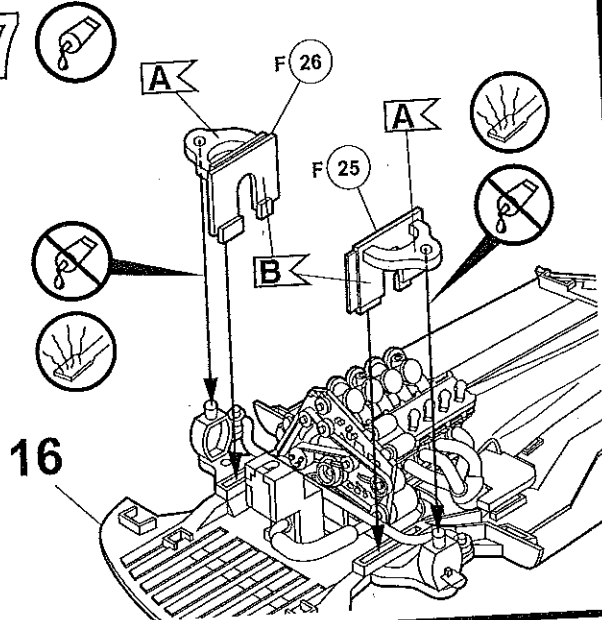
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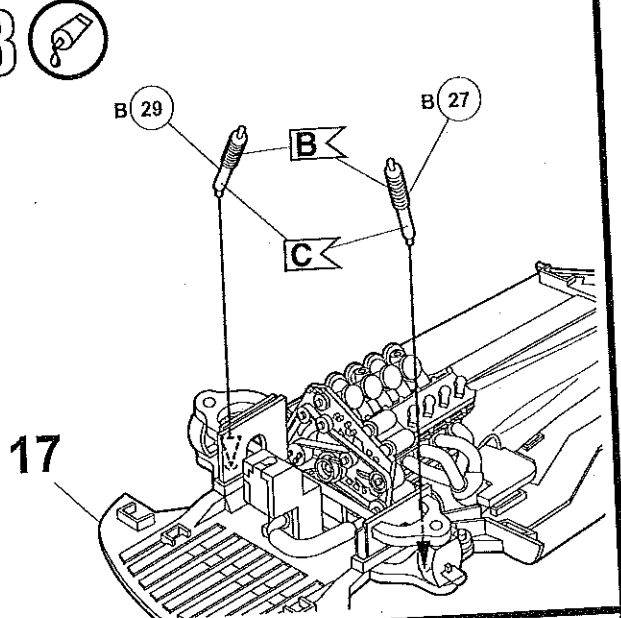
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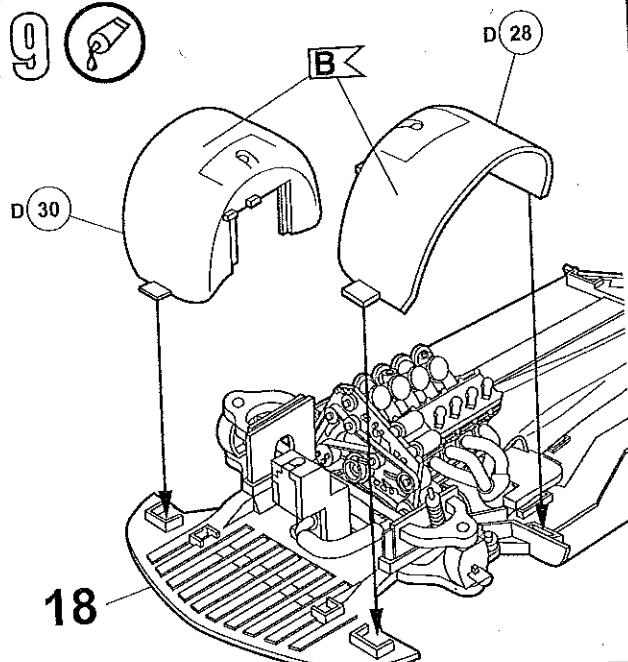
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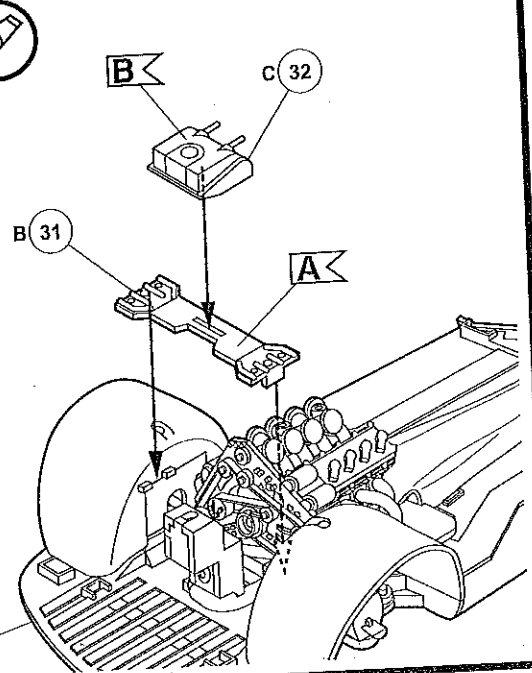
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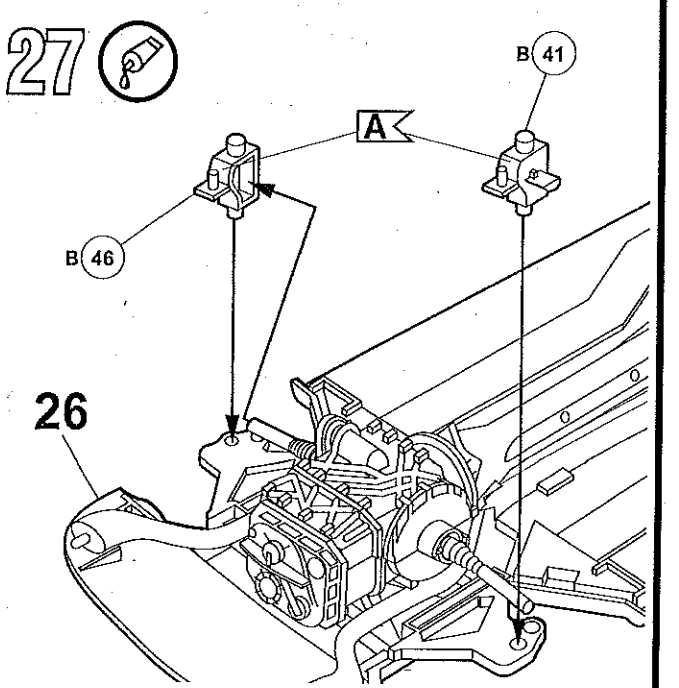
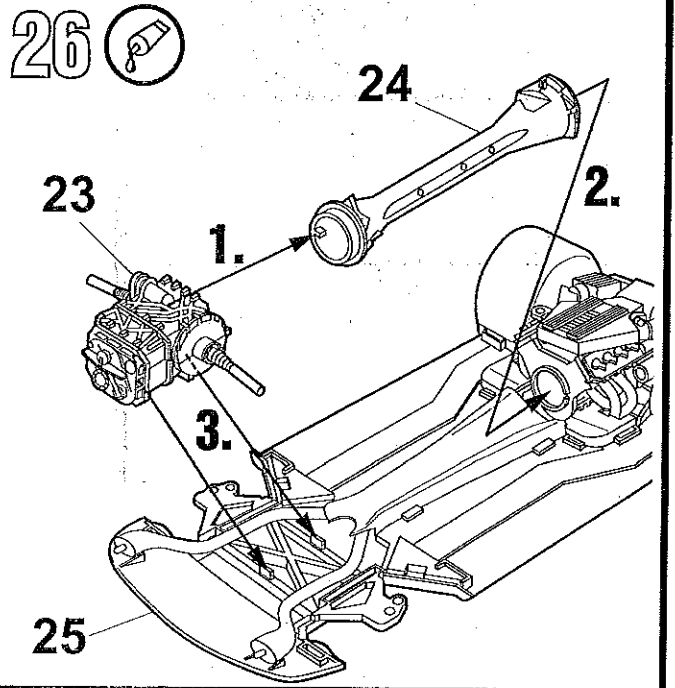
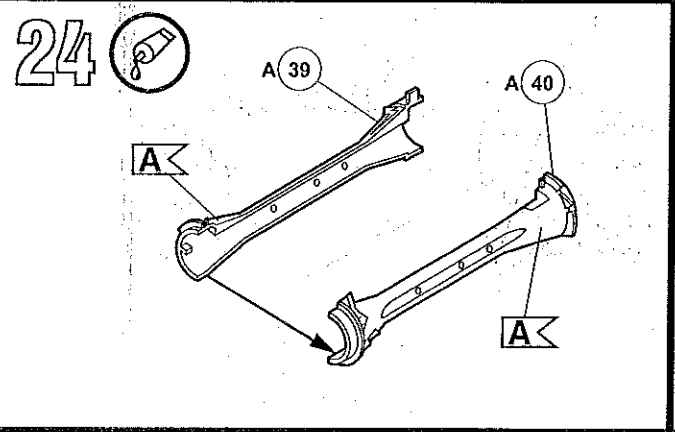
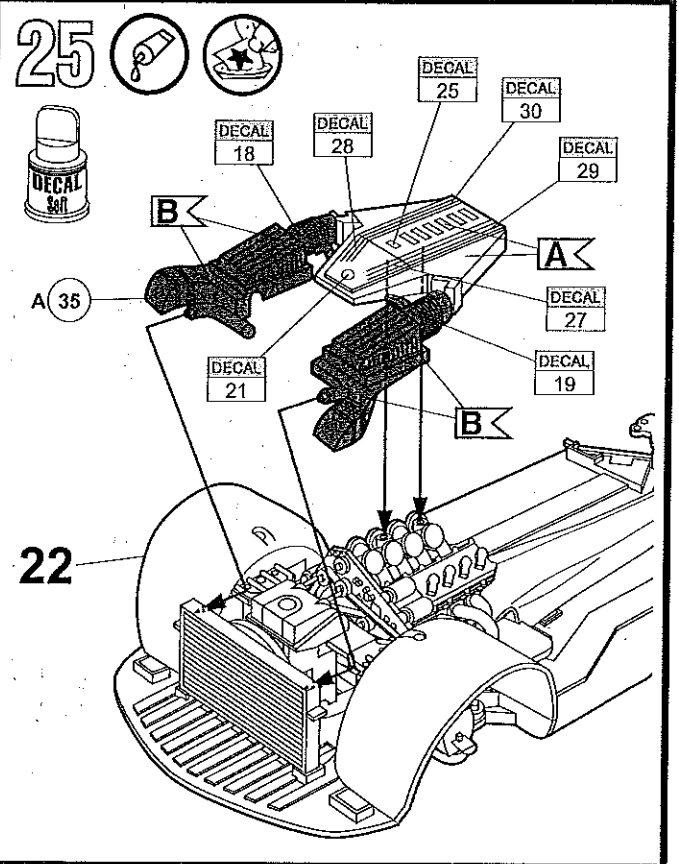
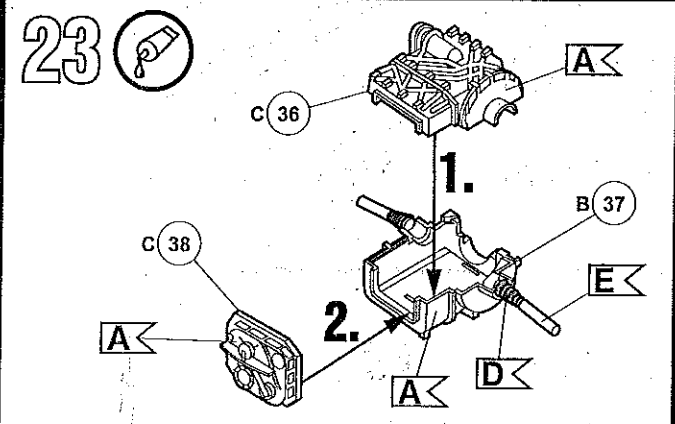
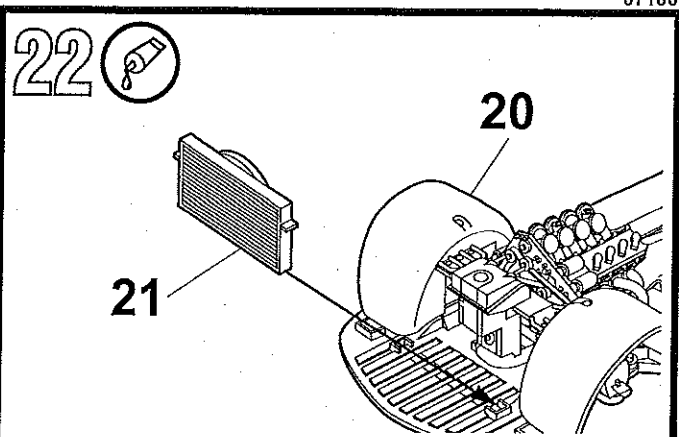
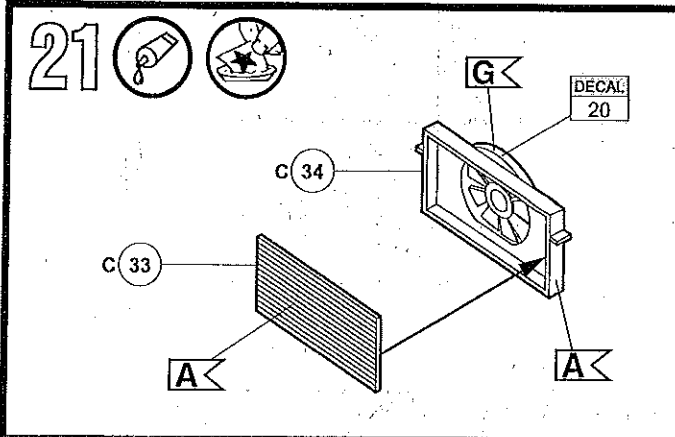


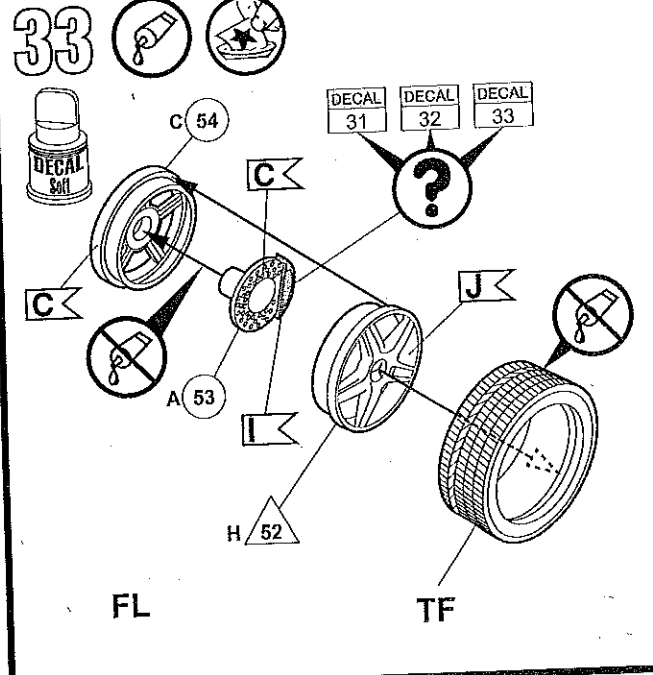
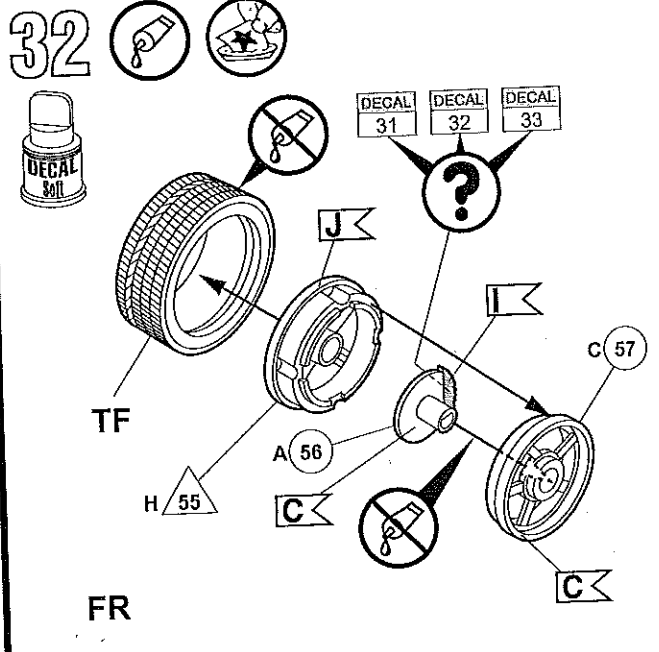
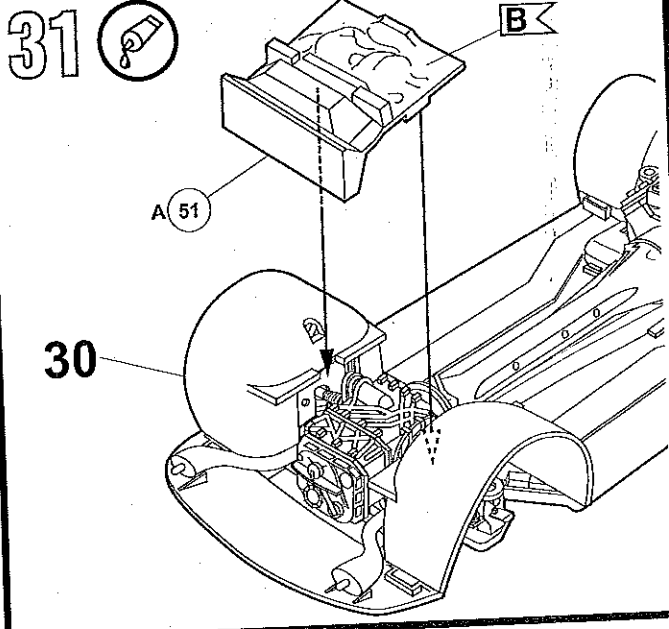
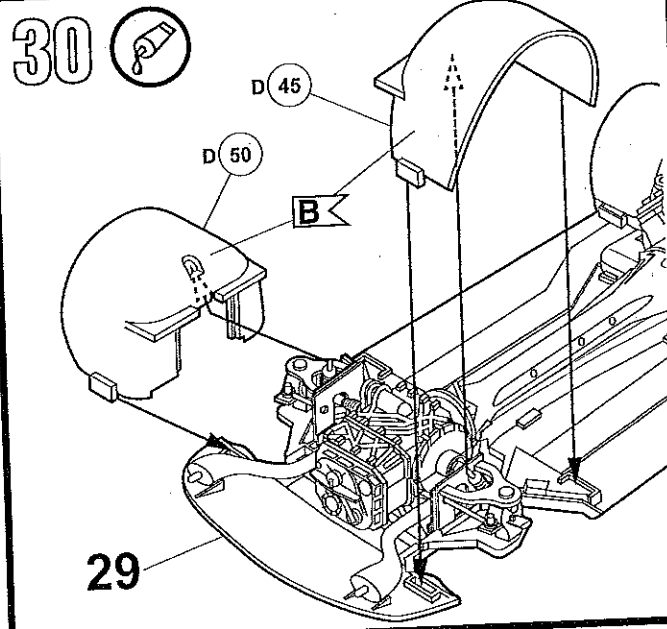
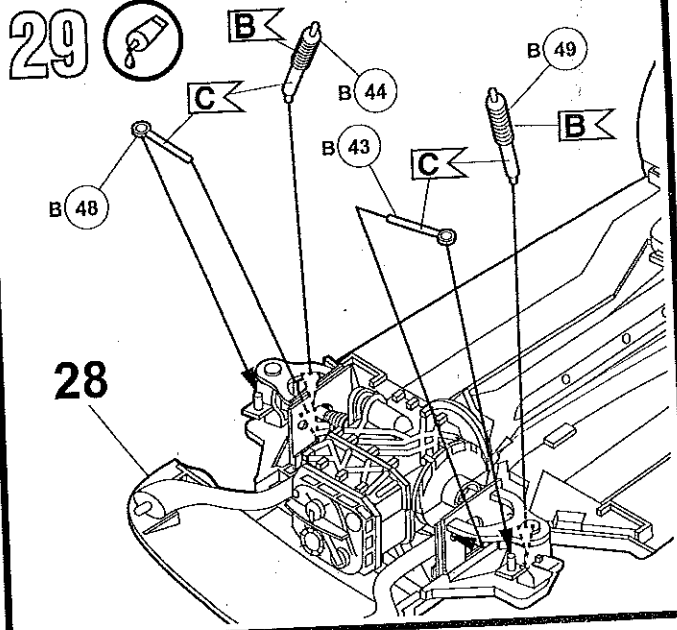
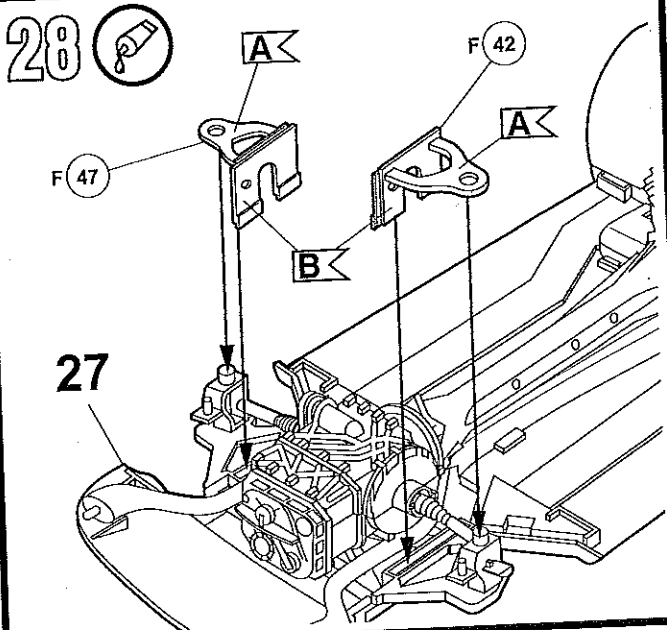
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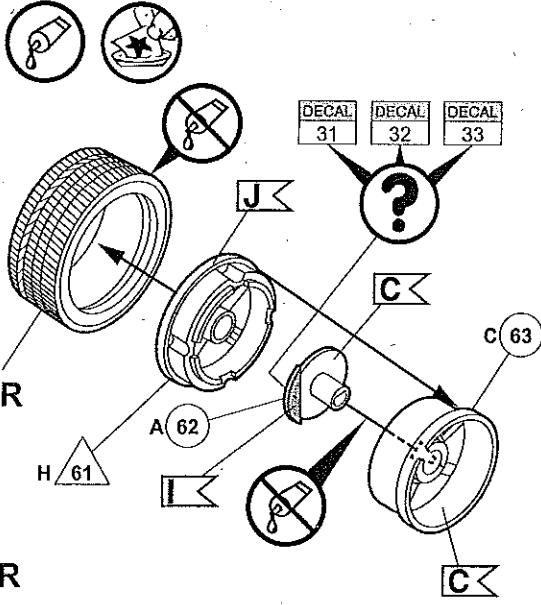
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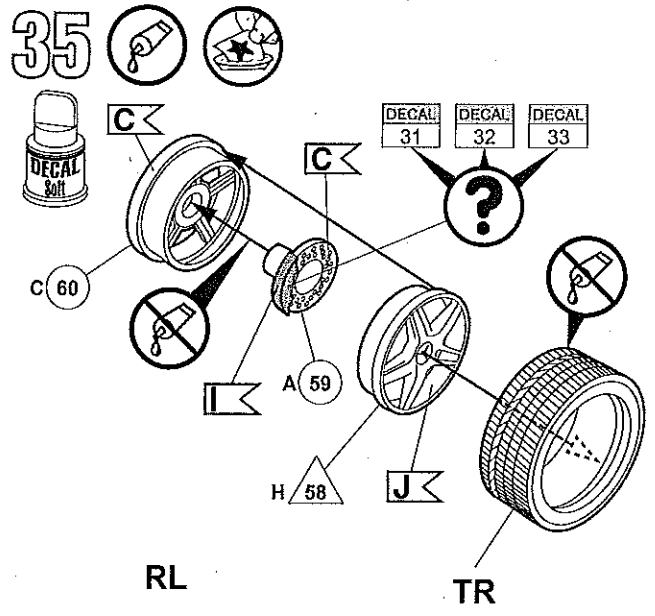




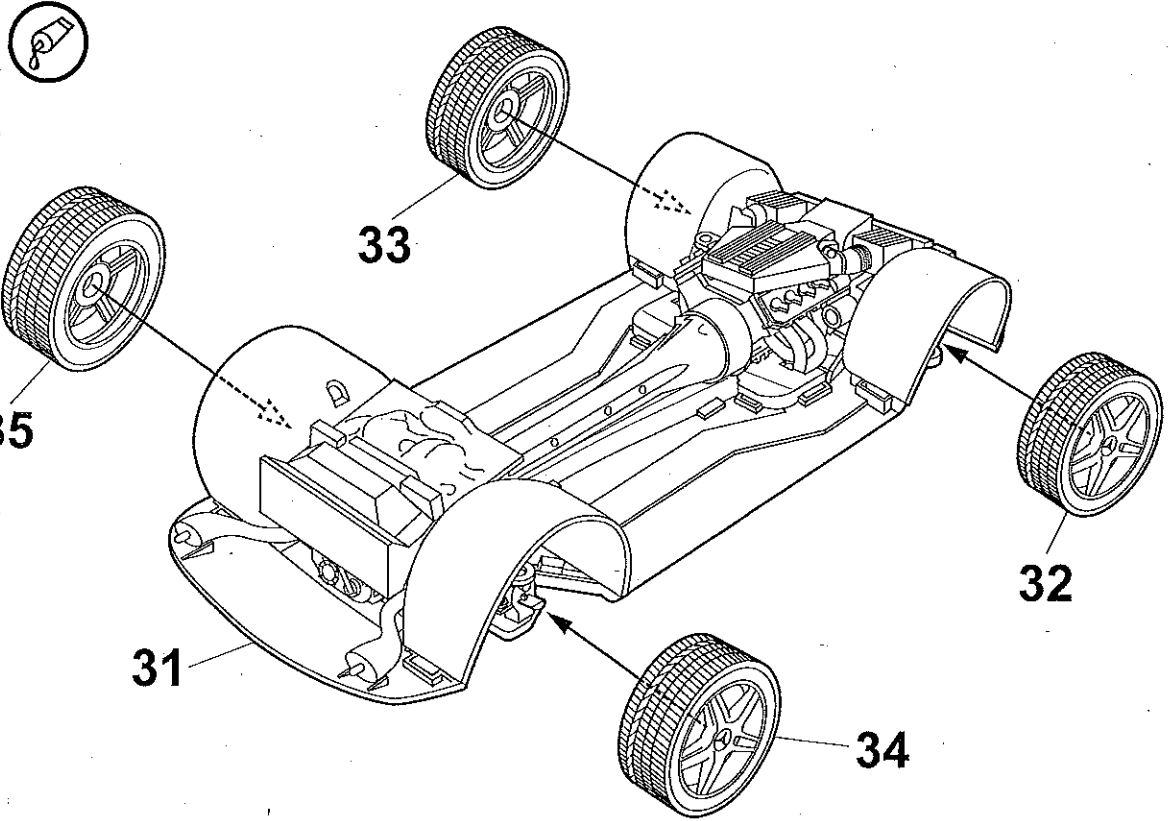
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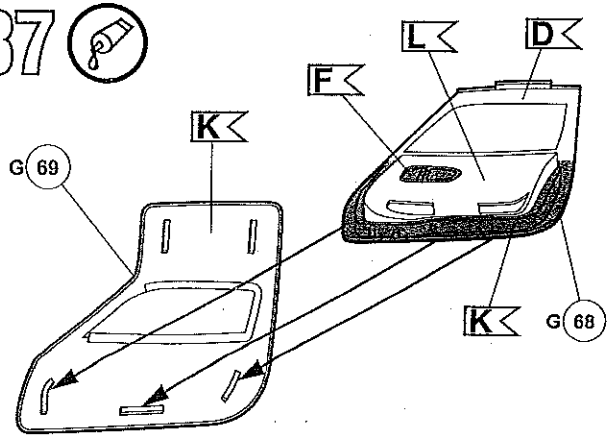
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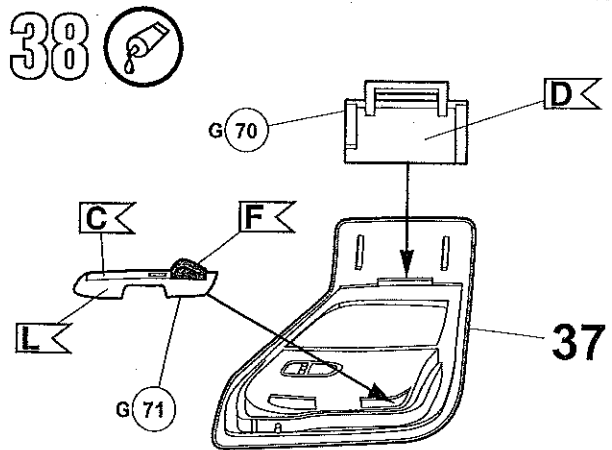
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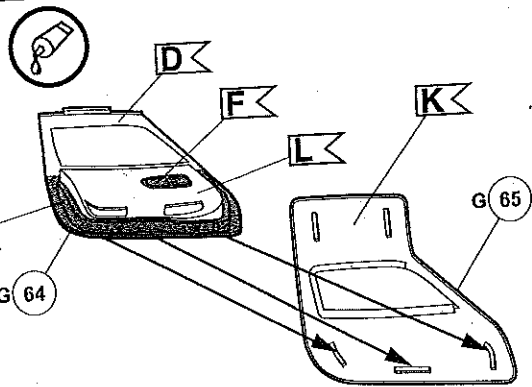
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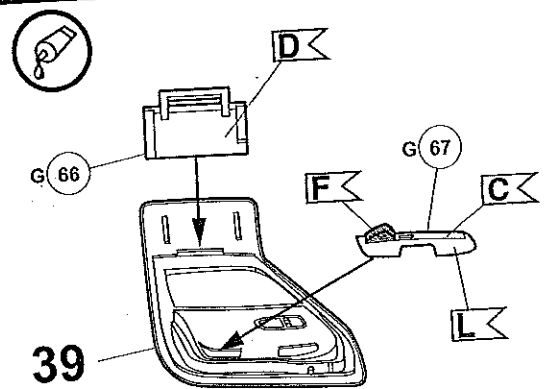
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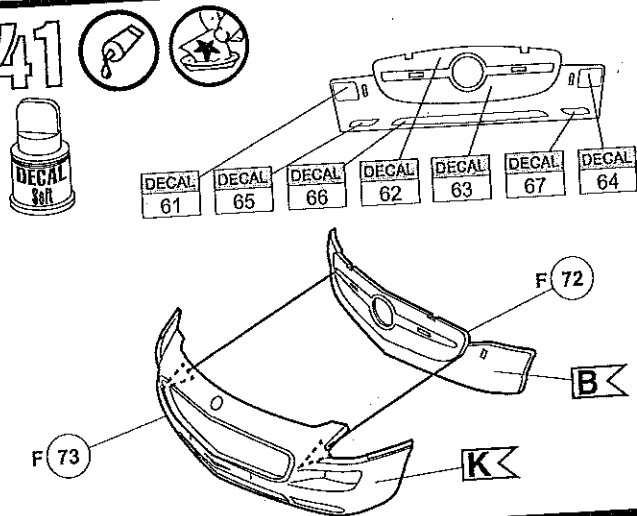
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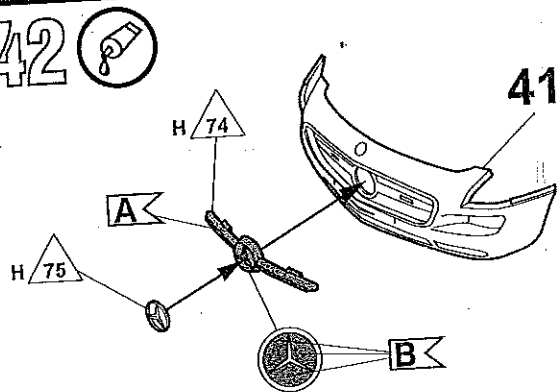
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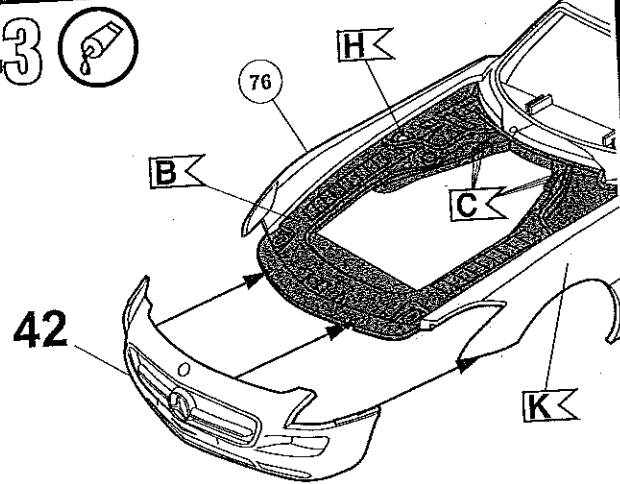
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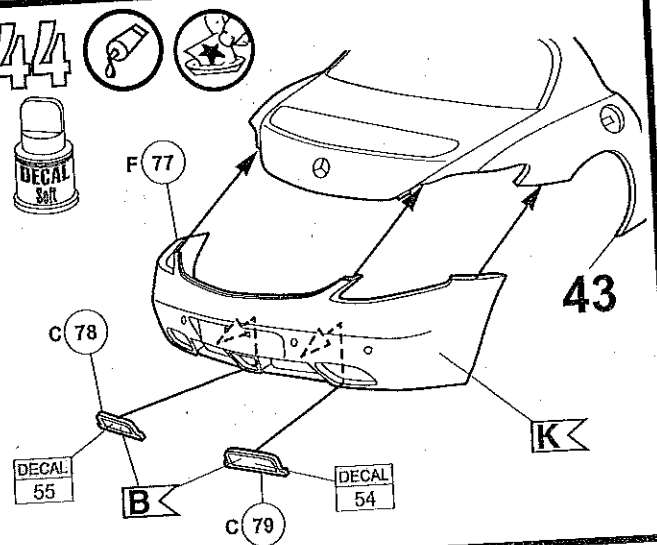
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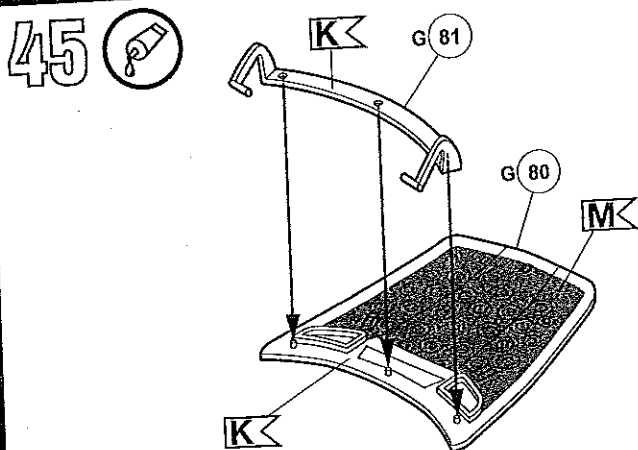
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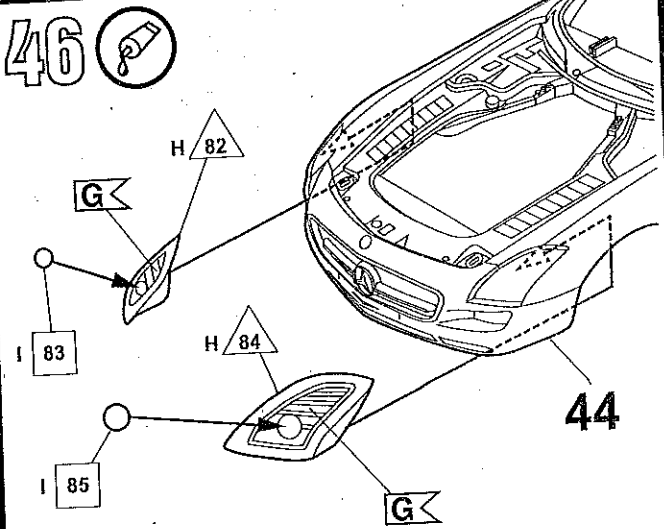
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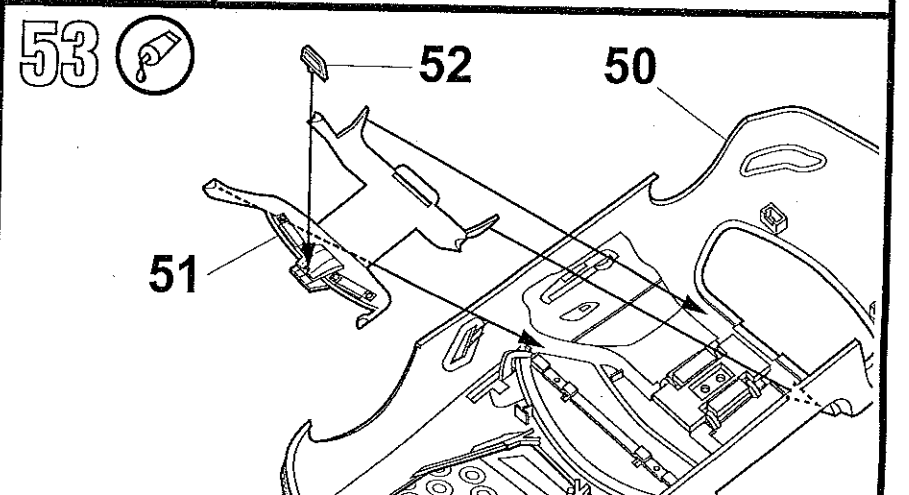
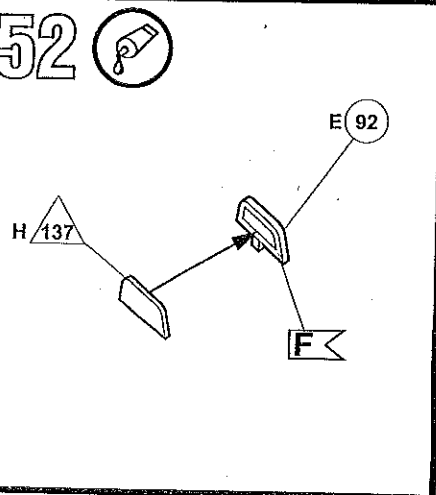
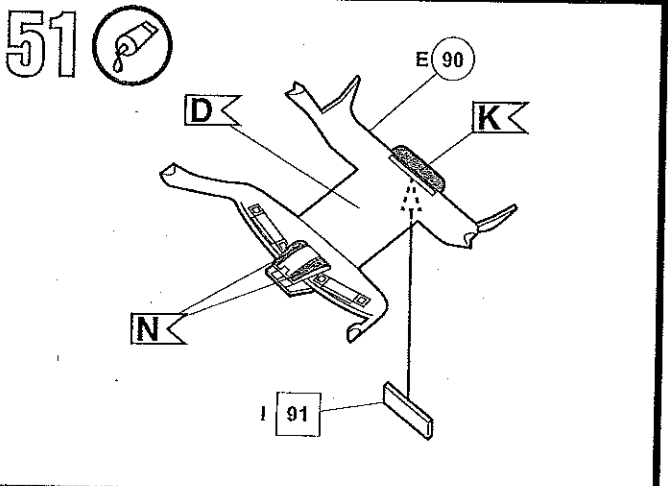
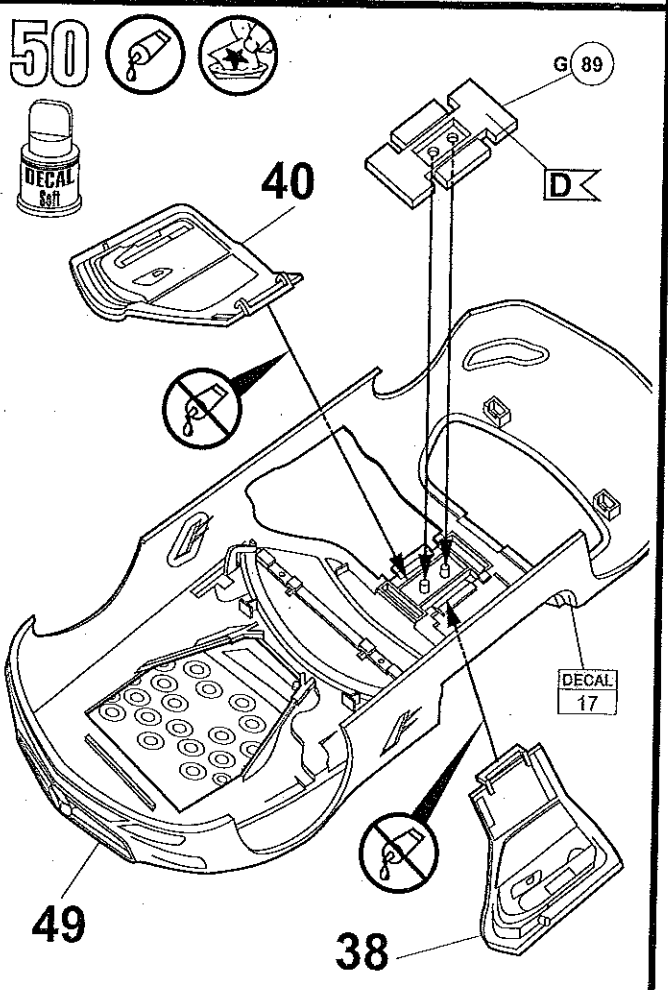
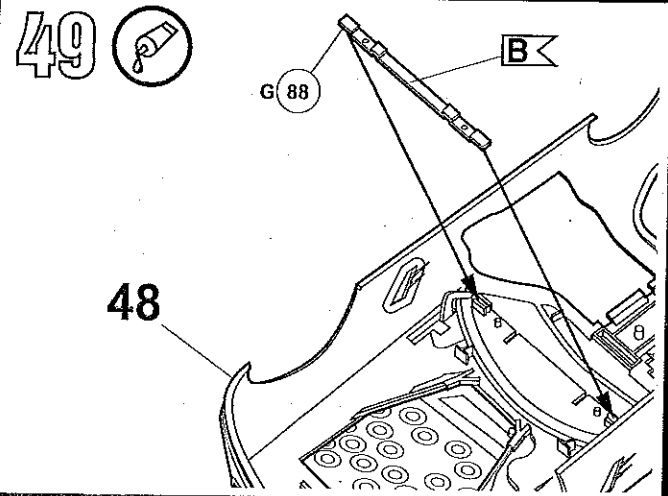
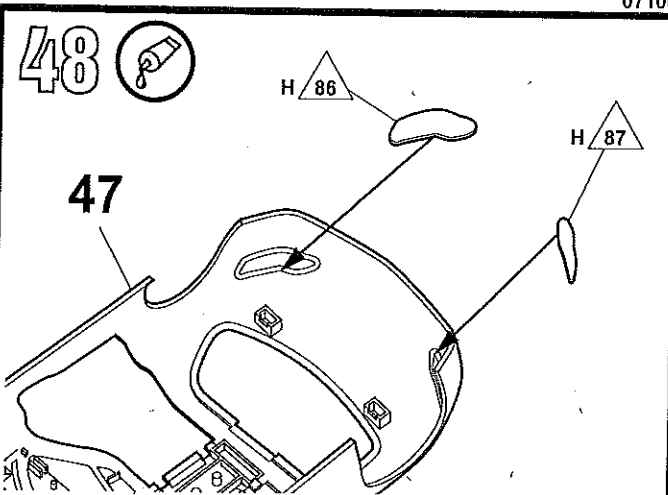
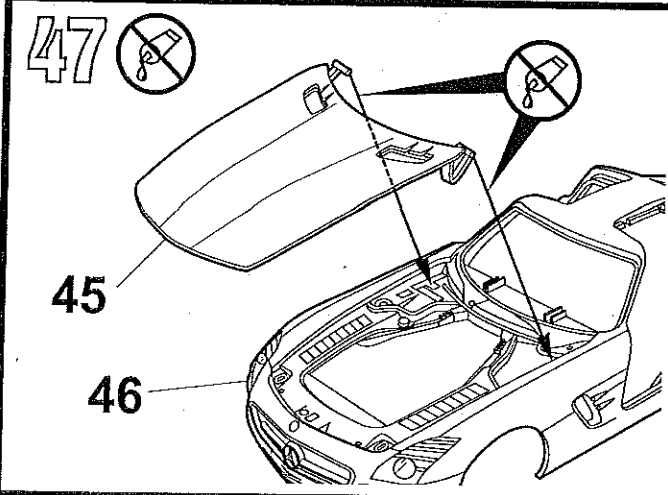


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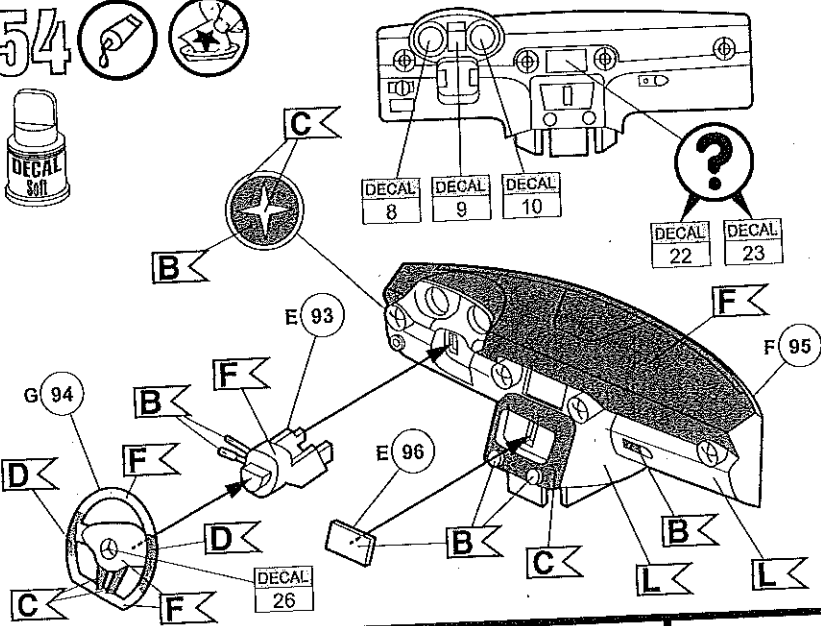


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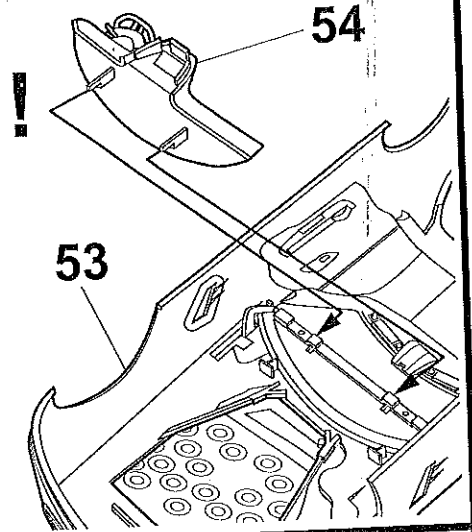




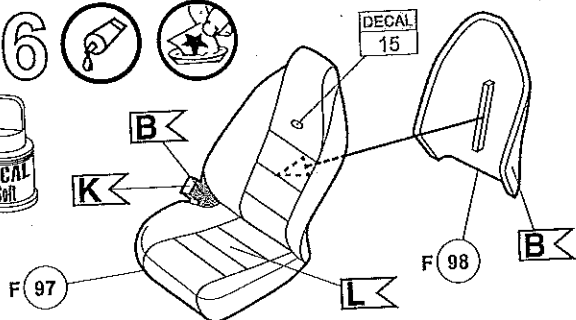
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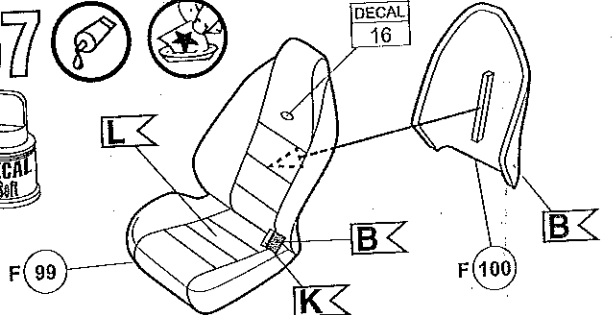
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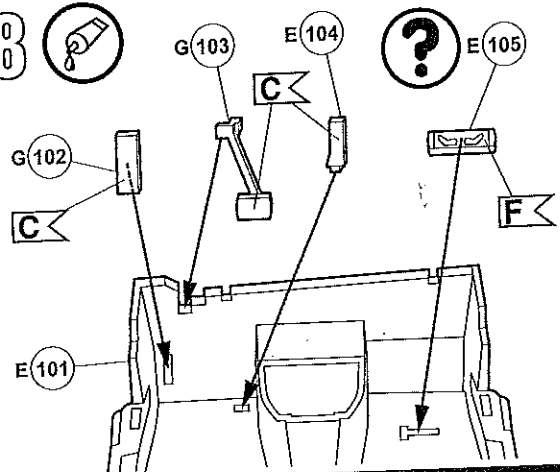
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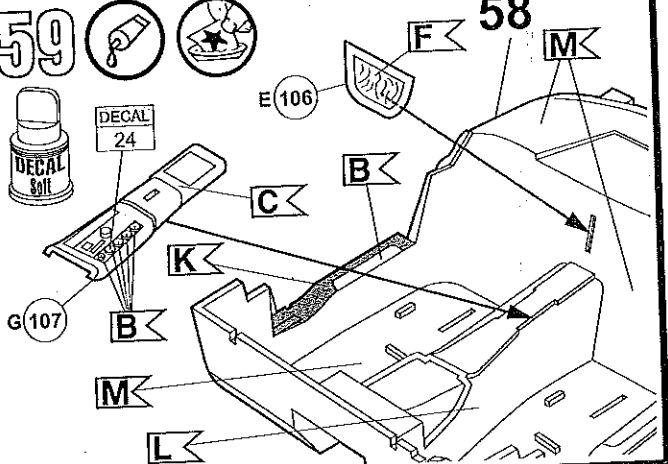
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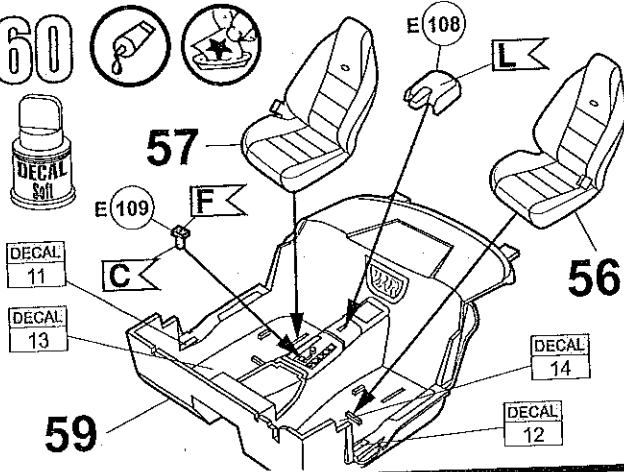
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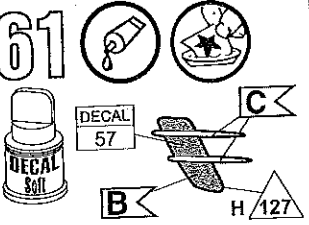
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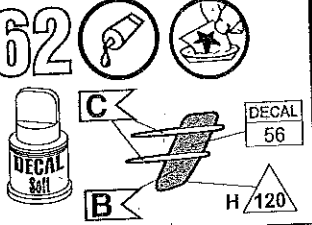
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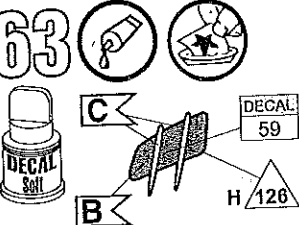
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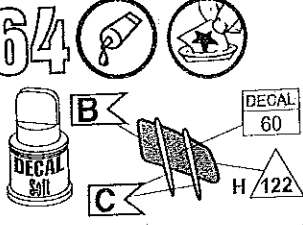
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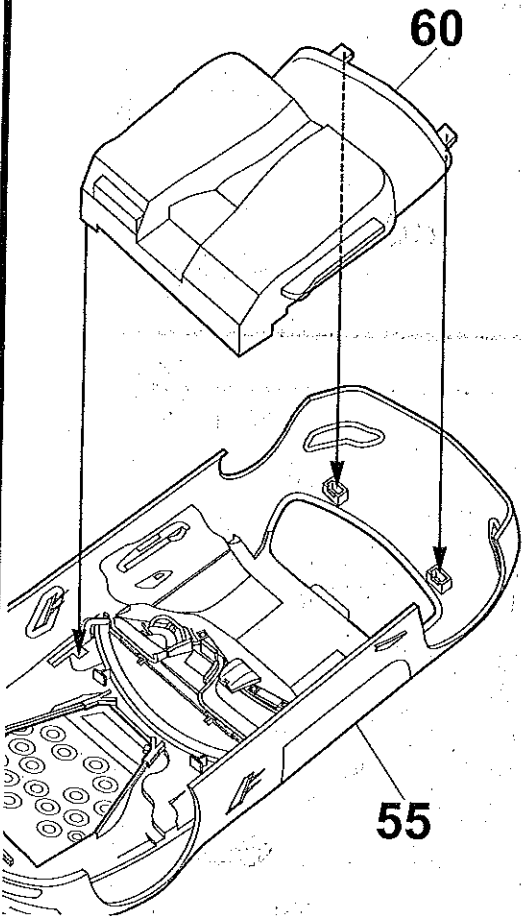
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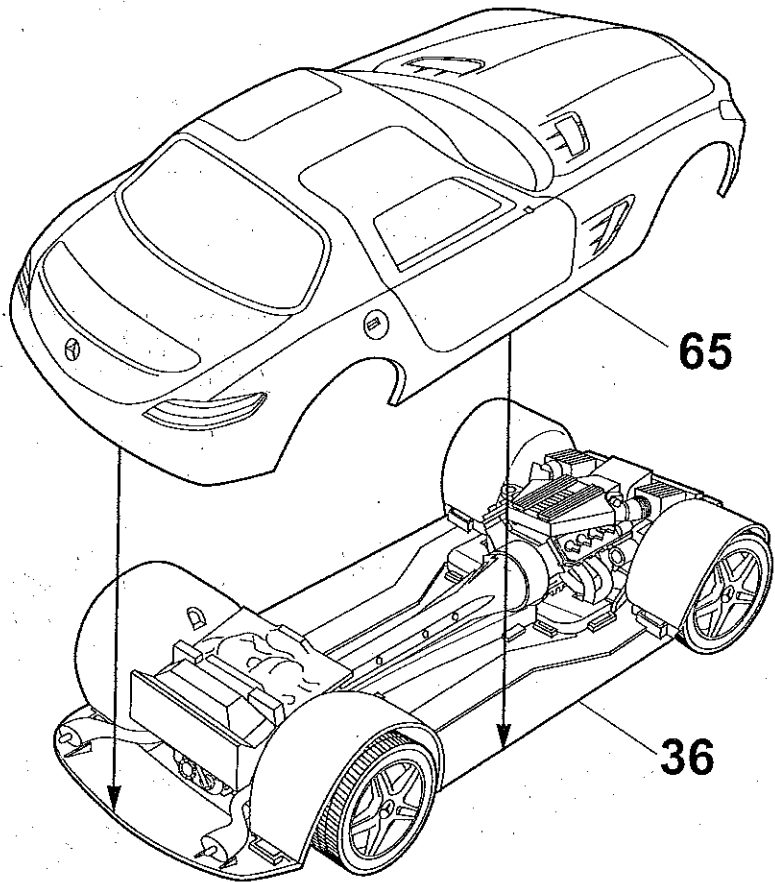
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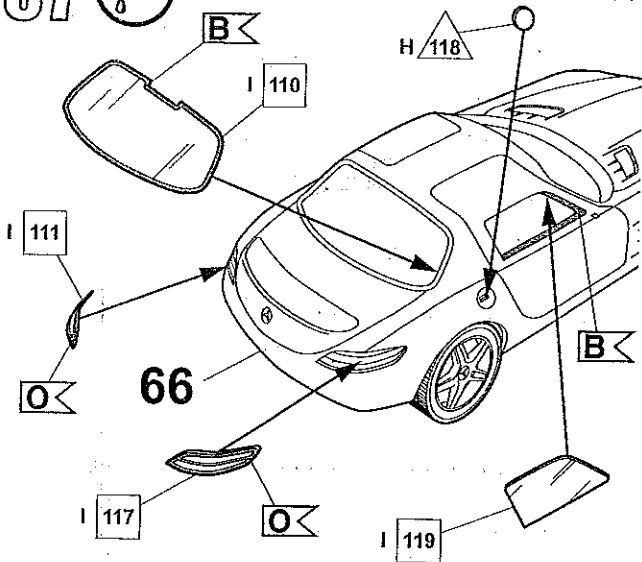
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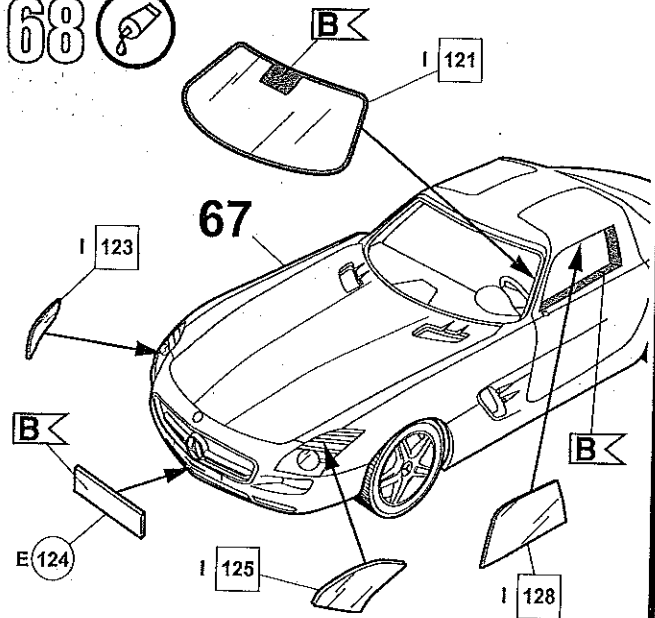
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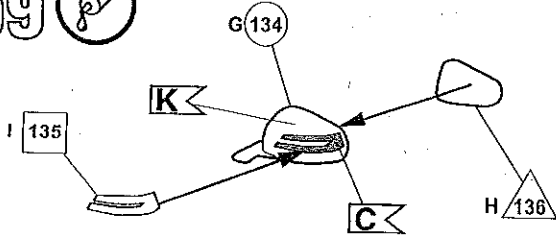
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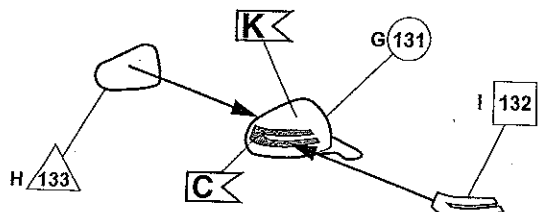
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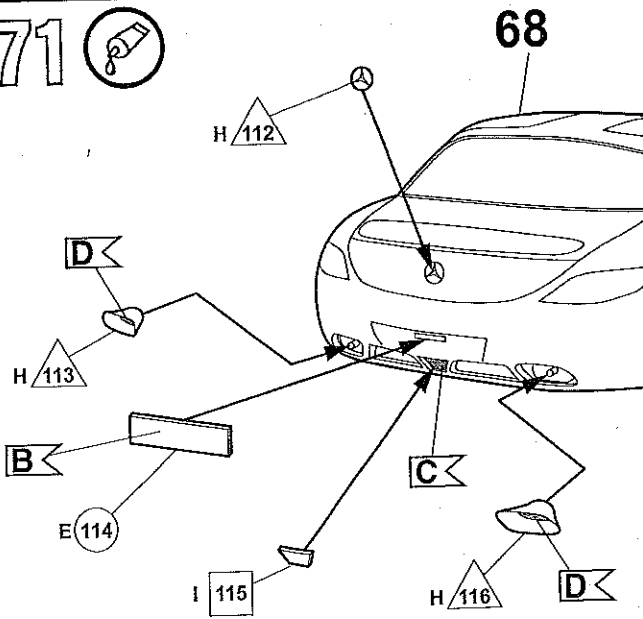
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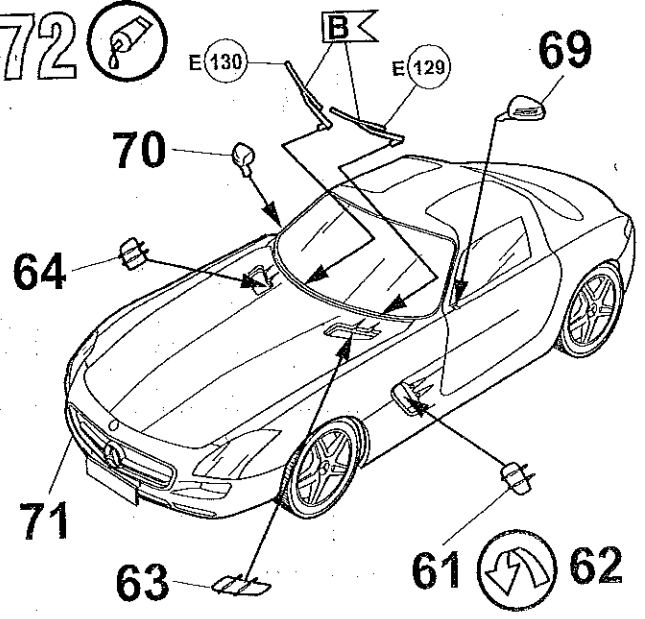
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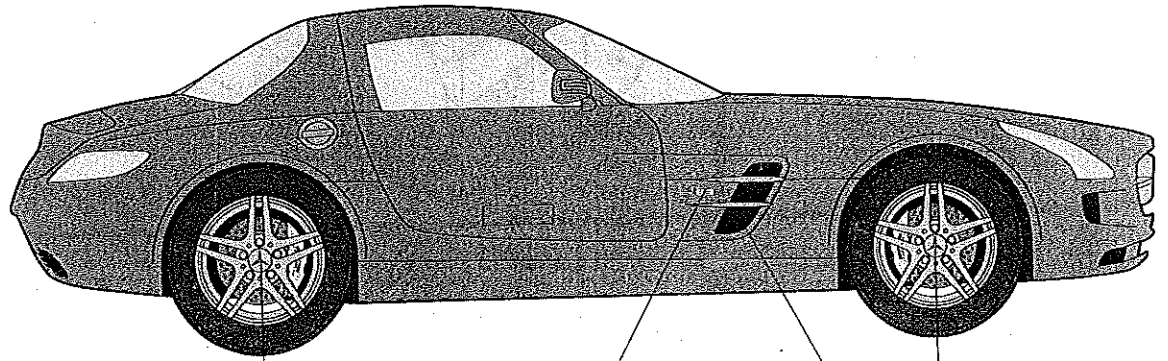
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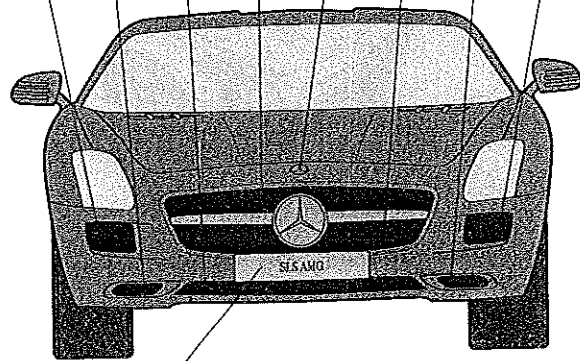
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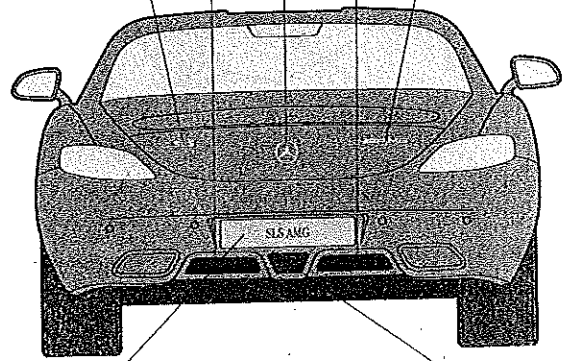


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